

A PHOTOGRAPHIC SURVEY OF THE
WILMINGTON & WELDON RAILROAD CORRIDOR

VOLUME I

(Ruins at Wilmington, NC, 2000-2001)

By

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Original Photographs by James C. Burke
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Figure 1

The retaining wall at the corner of Nutt and Hanover streets (November, 2000)



Figure 2

In this photograph, details of the masonry on the corner of the wall at the Nutt Street and Brunswick Street show that cracking and shifting have taken place. The stone base upon which the masonry rests can be seen in the lower left hand corner of the photograph. Separation of the concrete blocks on the crown of the corner is noticeable.



Figure 3

This photograph was taken from the sidewalk on Nutt Street looking towards the intersection of Nutt and Brunswick Streets. At this point, one can observe the wall being forced outward at its base. Interior elements of composite material are exposed between the cracks of surface material.



Figure 4

In this section of the wall facing Nutt Street, details of tile patterns are visible.



Figure 5

This photograph shows tile and slate wall surface. Some surface material has fallen off to expose loose interior elements.



Figure 6

This photograph is of a section of the same wall where roofing slates are visible. Holes are drilled in a few of these slates. For example, notice the hole in the upper left corner of the first slate on the left.



Figure 7

Separation of concrete blocks that crown the Nutt Street wall.



Figure 8

New wall constructed on the older stone wall as seen at the corner of Hanover Street and Nutt Street.



Figure 9

Close-up of granite in older section of same wall mentioned in Figure 8.



Figure 10

The northeast corner of Block 245 at the intersection of Hanover and Front Streets. Note how the stone section of this wall is buried.



Figure 11

Stonework and an iron rail in Block 231, in front of retaining wall where a public park is located.



Figure 12

This is the retaining wall at the site of the public park.



Figure 13

Mixed debris at retaining wall mentioned above. Note half-buried rail.



Figure 14

Metal and stone debris at same place mentioned above.



Figure 15

Section of a concrete slab in the same area (below the public park at the corner of Red Cross and Nutt Streets.



Figure 16

Iron railroad material can be found on the surface of the parking lot on Block 231 near Nutt Street.



Figure 17

Concrete footing for old ACL concourse under Front Street Bridge.



Figure 18

Unusual cut brick footing with iron connectors embedded in mortar in parking lot of Block 231 near the granite wall.



Figure 19

Another concrete footing for old ACL concourse under Front Street Bridge.



Figure 20

Buried section of foundation or footing for stairs in the parking lot of Block 231 near the granite wall.



Figure 21

Section of concrete slab at the same place as Figure 20.



Figure 22

Retaining wall in Block 231 near TV 3 dishes. Note the pipe that once when to a railroad water tower above wall.



Figure 23

This photograph shows some details of the same wall, crowned with concrete blocks.



Figure 24

This long shot of Block 231 was taken from the corner of Red Cross and Nutt Streets.



Figure 25

This long shot illustrates the different kinds of walls below the TV 3 Station.



Figure 26

A long shot of the same set walls shown in Figure 25 as seen from Nutt Street facing the Front Street Bridge.



Figure 27

The brickwork wall and stone wall at the same place as Figure 26. This view is from the Front Street Bridge facing Nutt Street.



Figure 28

Note that the stone wall has been repaired with bricks above and brick fragments in the cracks below.



Figure 29

This is a close-up of the repair work of the stone wall. Note blackening of the stone wall.



Figure 30

This photograph was taken of the same wall structures from a position on the Front Street Bridge at its north end. Note how brick structure is made of three walls, including the half buried east side. The wall of conglomerate material extends behind the brick structure.



Figure 31

The brick walls are shifting apart.



Figure 32

Conglomerate wall recedes behind brick foundation of the Front Street Bridge.



Figure 33

This is a close-up of conglomerate material.



Figure 34

Brick fragments are mixed in conglomerate.



Figure 35

This is another view of conglomerate material in same wall.



Figure 36

This photograph is of the Front Street Bridge as viewed from Nutt Street.



Figure 37

A view of brickwork paving inside retaining wall at Public Park on Block 231.



Figure 38

A long shot of public park area.



Figure 39

The retaining wall at the public park.



Figure 40

Concrete structure inset on wall in parking lot.



Figure 41

Old brick wall on the southeast side of the embankment of the Front Street Bridge.



Figure 42

Detail of old brickwork on the same wall as Figure 41.



Figure 43

North side under the Front Street Bridge



Figure 44

South side under the Front Street Bridge



Figure 45

Long shot taken from a position at the center of the granite wall on the north side of Block 231 facing the south side of the Front Street Bridge.



Figure 46

View of (north) Nutt Street taken from Front Street and Red Cross Street.



Figure 47

South side of Front Street Bridge.



Figure 48

South side of Front Street facing Front Street Bridge.



Figure 49

Old buttress of stone retaining wall with more recent concrete structures at the north side of the Front Street Bridge.



Figure 50

A different view of the same wall as shown in Figure 49.



Figure 51

Stone and concrete structures under the south side of the Front Street Bridge.



Figure 52

Another view of the same structures as in Figure 51.

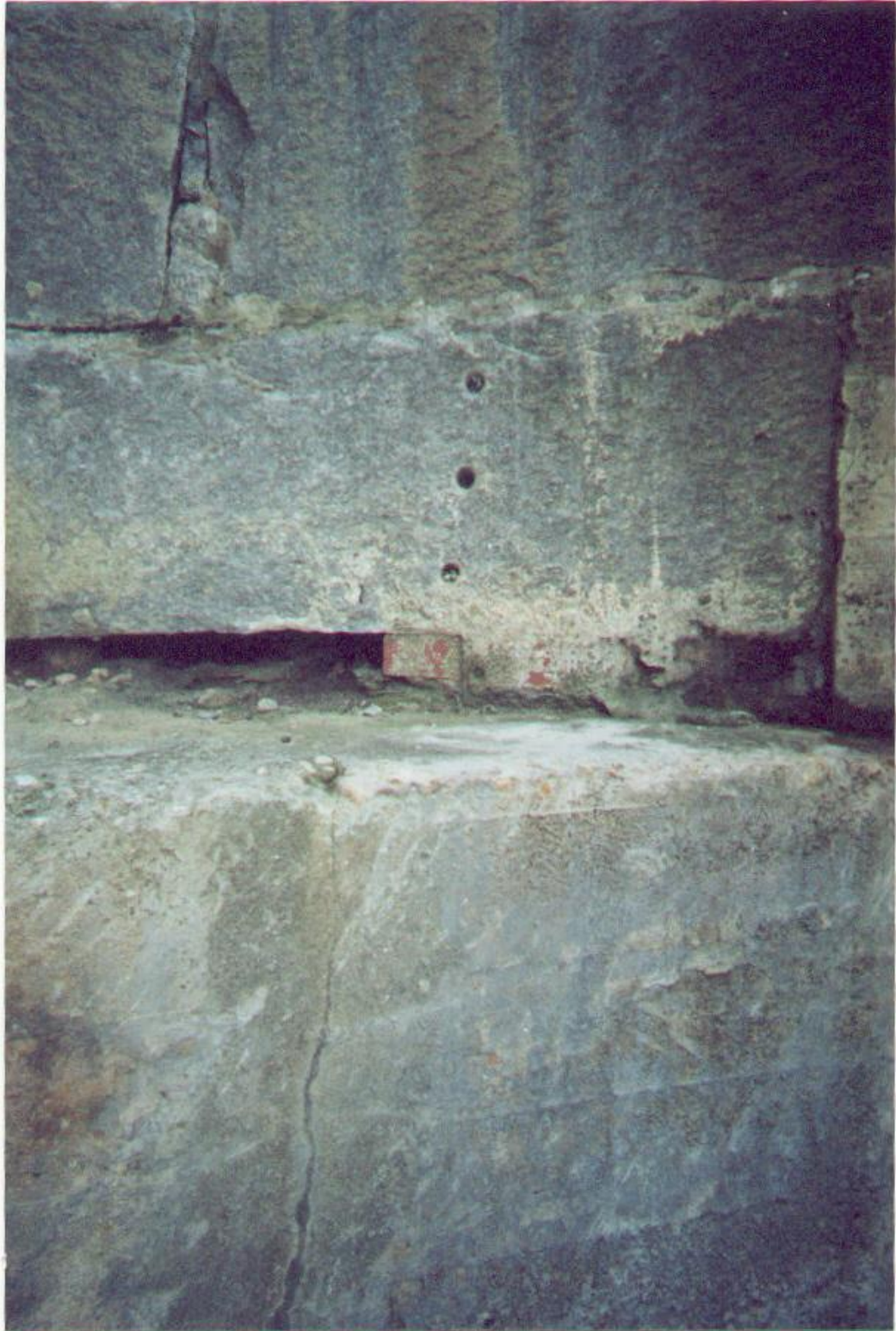


Figure 53

Close-up shot of the stone section of same wall as in Figure 51 and 52. Note drilled holes for iron connector for earlier bridge.



Figure 54

Concrete piers of the old concourse on the south side of Front Street Bridge.



Figure 55

Ruins of rear wall of car shop in northeast corner of Block 247.



Figure 56

Long shot of the same wall as seen in Figure 55, on Hanover Street side.



Figure 57

Note granite foundation below old car shop walls.



Figure 58

View of brickwork on the car shop wall.



Figure 59

Shot of car shop wall looking in side Block 247 looking east to 3rd Street.



Figure 60

Close-up of brick and granite construction of car shop wall.



Figure 61

Note the two types of granite used in car shop foundation, and also note the cross at one foot on the light colored stone resting on the darker stone.



Figure 62

Car shop ruins at far northeast corner of Block 247 near the 3rd Street Bridge.



Figure 63

Detail of brickwork on west end of the car shop wall. Hanover Street can be seen on the other side of this wall.



Figure 64

View of the ruins showing the division between the two car shops.



Figure 65

Long shot of car shop walls from inside Block 247.



Figure 66

Brickwork at northeast corner of Block 247.



Figure 67

View looking east from 3rd Street Bridge before paved over as parking.



Figure 68

View looking west from 4th Street Bridge during parking lot construction.



Figure 69

Long shot of railroad cut taken from 3rd Street Bridge looking east before construction.



Figure 70

View from 3rd Street Bridge looking southeast before parking lot construction.

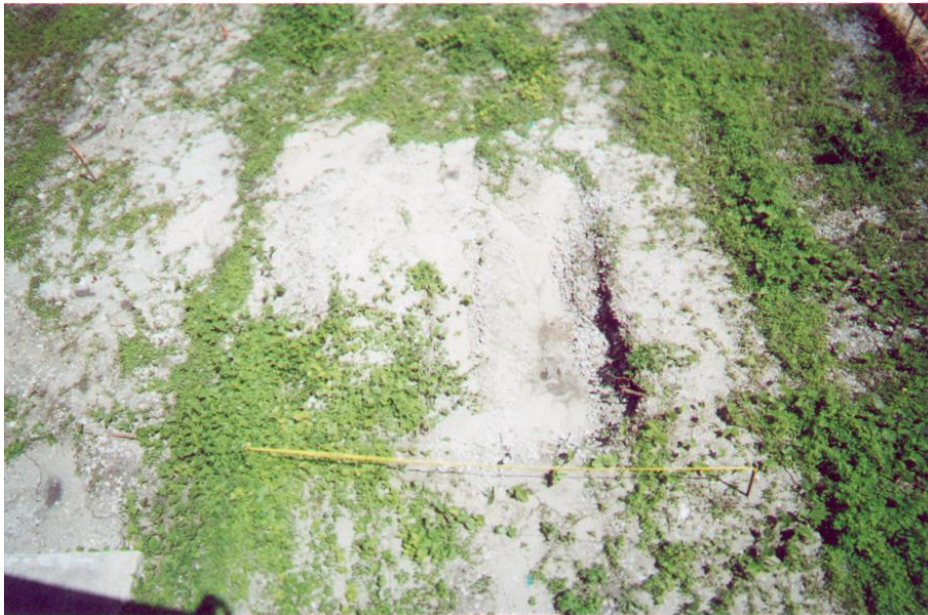


Figure 71

View below 3rd Street Bridge on east side – excavation of rail bed for drainage pipe.



Figure 72

View from 4th Street Bridge looking west – construction.



Figure 73

Retaining wall as seen from the south side of the 4th Street Bridge.



Figure 74

An ancient stream flows in the inclined plane where tracks to the river once ran, under the 3rd Street Bridge.



Figure 75

View from the 6th Street Bridge looking west through the railroad cut.



Figure 76

View from the 6th Street Bridge looking east through the railroad cut.



Figure 77

View from McRae Street looking west through the railroad corridor above the cut.



Figure 78

The pavement on McRae Street is patched where tracks once crossed the street.



Figure 79

View from McRae Street looking east on the opposite side of street as Figure 77.



Figure 80

The same location as Figure 79, viewed from a different position.



Figure 81

Note Blue house in the distance, the same house as in Figure 79. This photograph was taken from the driveway of Dorothy B. Johnson School, which is located at what was the lower part of the ACL upper yards. The community of Love Grove is above the site of ACL upper yard



Figure 82

View from inside the railroad corridor west of McRae Street.



Figure 83

View of the railroad corridor where it once crossed King Street near Love Grove.



Figure 84

Railroad crossing sign near the same site as Figure 83.

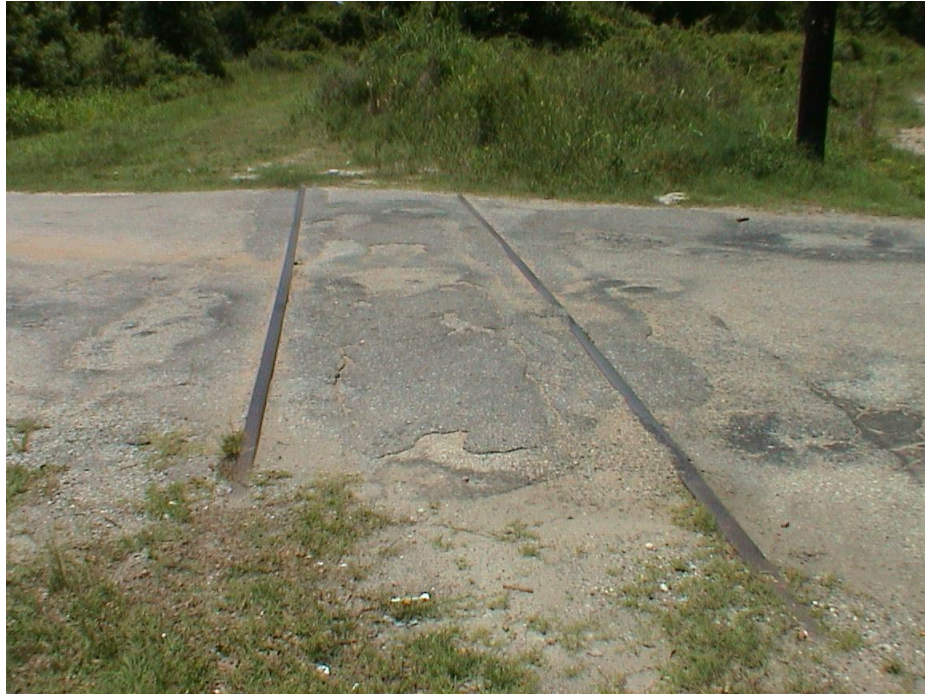


Figure 85

These tracks remain in King Street.



Figure 86

These tracks at the entrance of the Love Grove community are still in use.



Figure 87

These tracks near the entrance to Love Grove loop around Wilmington to the ports.



Figure 88

These same tracks as in Figure 87 cross Burnt Mill Creek to the east.



Figure 89

The track at Love Grove turns north to cross Smith Creek.



Figure 90

This sign is all that marks the present-day track crossing the only road into Love Grove.



Figure 91

Tracks on the north side of Smith Creek on the route of the Wilmington & Weldon R. R.



Figure 92

In 1837, the first train from Wilmington traveled the 10 miles of completed track.



Figure 93

The railroad crossing on Division Drive north of Wilmington.



Figure 94

The tracks continue north into Wrightsboro alongside Blue Clay Road.



Figure 94

View looking south on Blue Clay Road towards Wilmington. The sawmill in the right side of this photograph is no longer in operation.



Figure 95

View of the tracks looking south as they approach Castle Hayne.



Figure 96

Tracks cross Hwy 132 at Castle Hayne.



Figure 97

Tracks on the north side of the Hwy 132 crossing at Castle Hayne.



Figure 98

Abandoned houses on Orange Street in Castle Hayne opposite the tracks.



Figure 99

At the northern end of Orange Street in Castle Hayne, this curve marks the end of the line. The original Wilmington & Weldon track would have continued straight and would have crossed the Northeast Cape Fear River beyond the woods. The track beyond the present-day curve services industries located on the south bank of the river. It ends before I-40. In 1837, the train would take passengers to this point, about a one-hour ride, at which time they would cross the river and board stages waiting on the other side of the river. As more tracks were put down, the stage route was shortened. The railroad was completed in 1840.



Figure 100

The railroad bridge over the Northeast Cape Fear River is no longer in use.